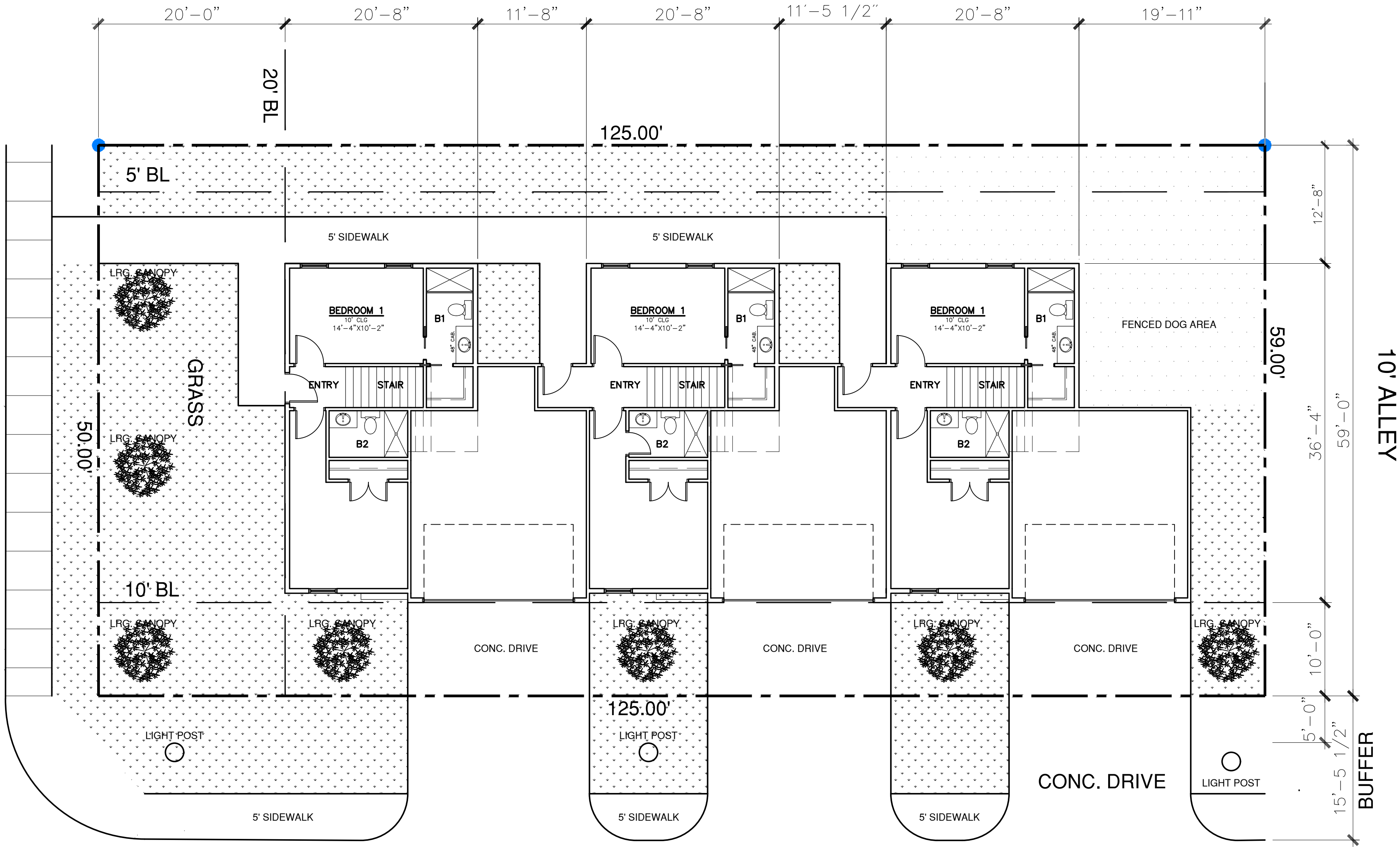


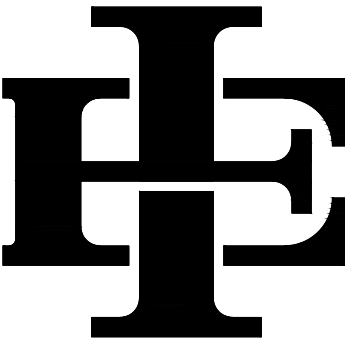
WAYSIDE AVENUE



W. CANTEY STREET

PLOT PLAN

SCALE: 1/4" = 1'-0"



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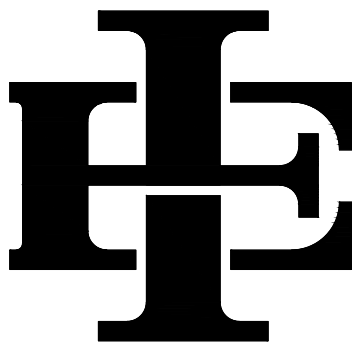
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OWNER INFORMATION

2800 WAYSIDE AVENUE
FORT WORTH, TX 76110
LOT:1&2 BLOCK: 17

ADDRESS:

ISSUE DATE	10.02.2024
DRAFTSPERSON	MEM
PROJECT NO.	000
PLAN NUMBER	2339
SHEET NUMBER	A-1



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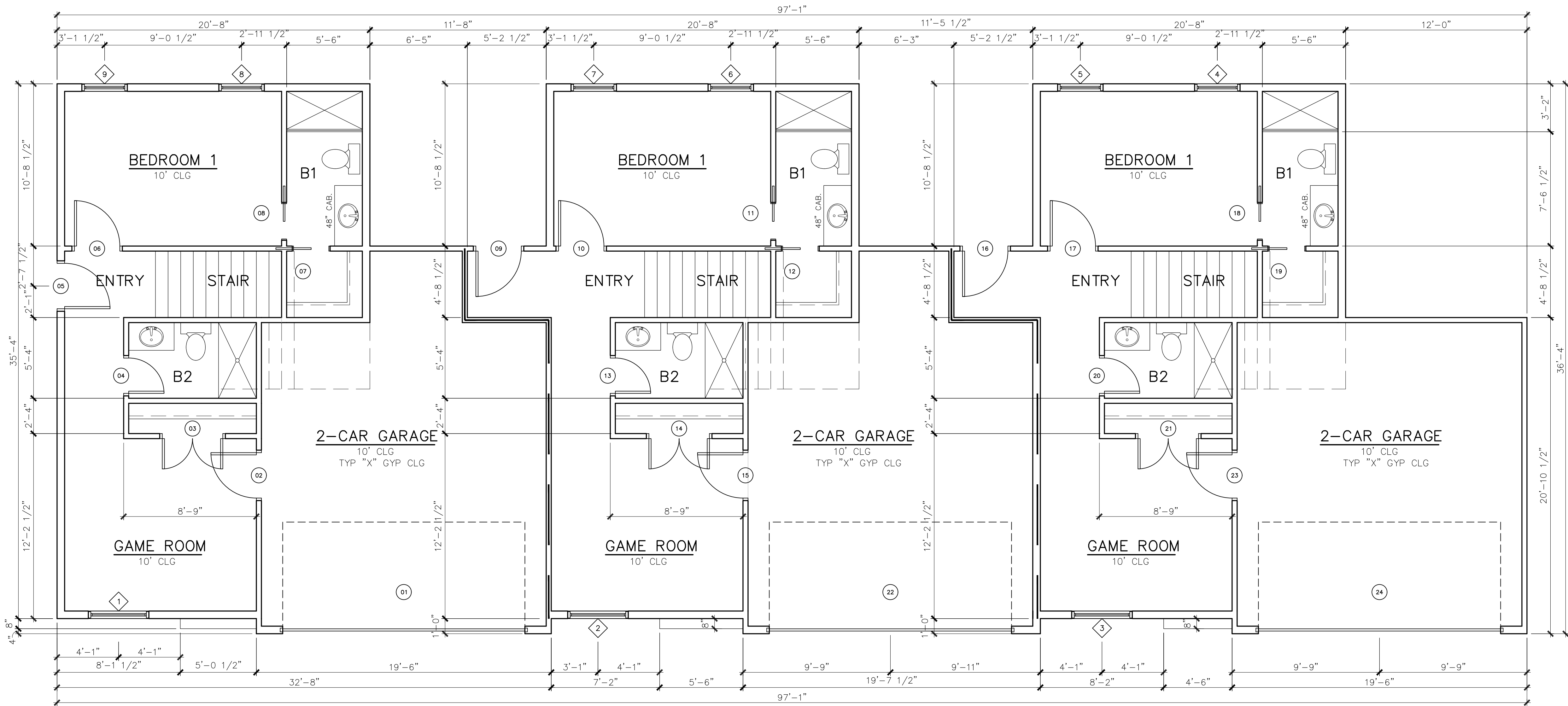
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PLAN NUMBER
2339

SHEET NUMBER

A-2

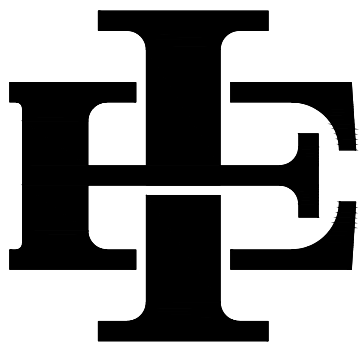


SQ. FOOTAGES – UNIT 1		
First Floor	545	SQ. FT.
Second Floor	682	SQ. FT.
Third Floor	739	SQ. FT.
Total Living Area	1966	SQ. FT.
Total Slab	545	SQ. FT.
2nd Floor Balcony	62	SQ. FT.
Garage	398	SQ. FT.
Total Under Roof	2426	SQ. FT.

SQ. FOOTAGES – UNIT 2-3		
First Floor	618	SQ. FT.
Second Floor	682	SQ. FT.
Third Floor	739	SQ. FT.
Total Living Area	2039	SQ. FT.
Total Slab	618	SQ. FT.
2nd Floor Balcony	62	SQ. FT.
Garage	398	SQ. FT.
Total Under Roof	2499	SQ. FT.

FIRST FLOOR PLAN

SCALE: 1/4" = 1'-0"



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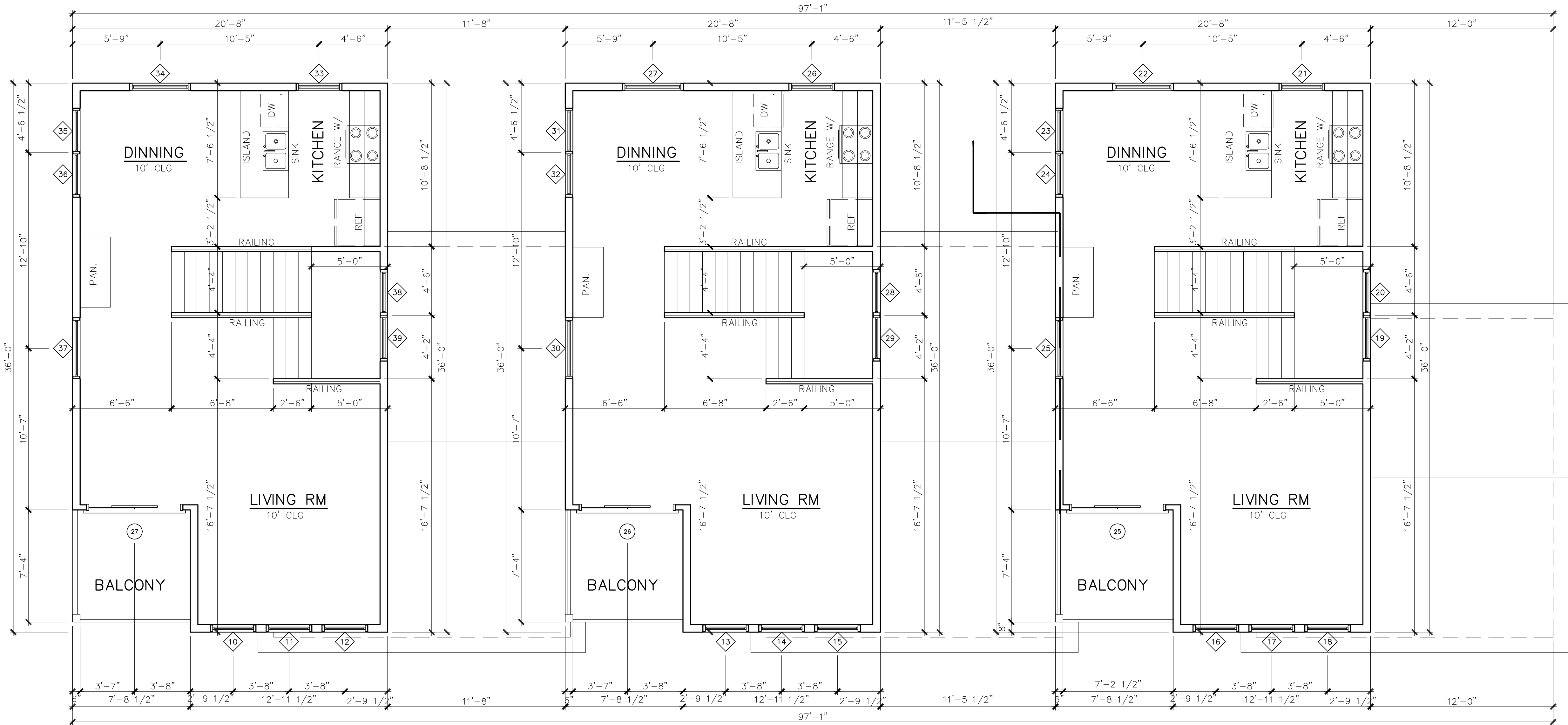
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PLAN NUMBER
2339

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SECOND FLOOR PLAN

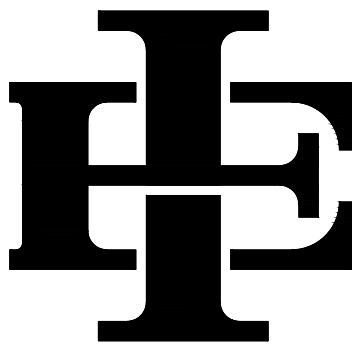
SCALE: 1/4" = 1'-0"

SECOND FLOOR PLAN

SCALE: 1/4" = 1'-0"

SECOND FLOOR PLAN

SCALE: 1/4" = 1'-0"



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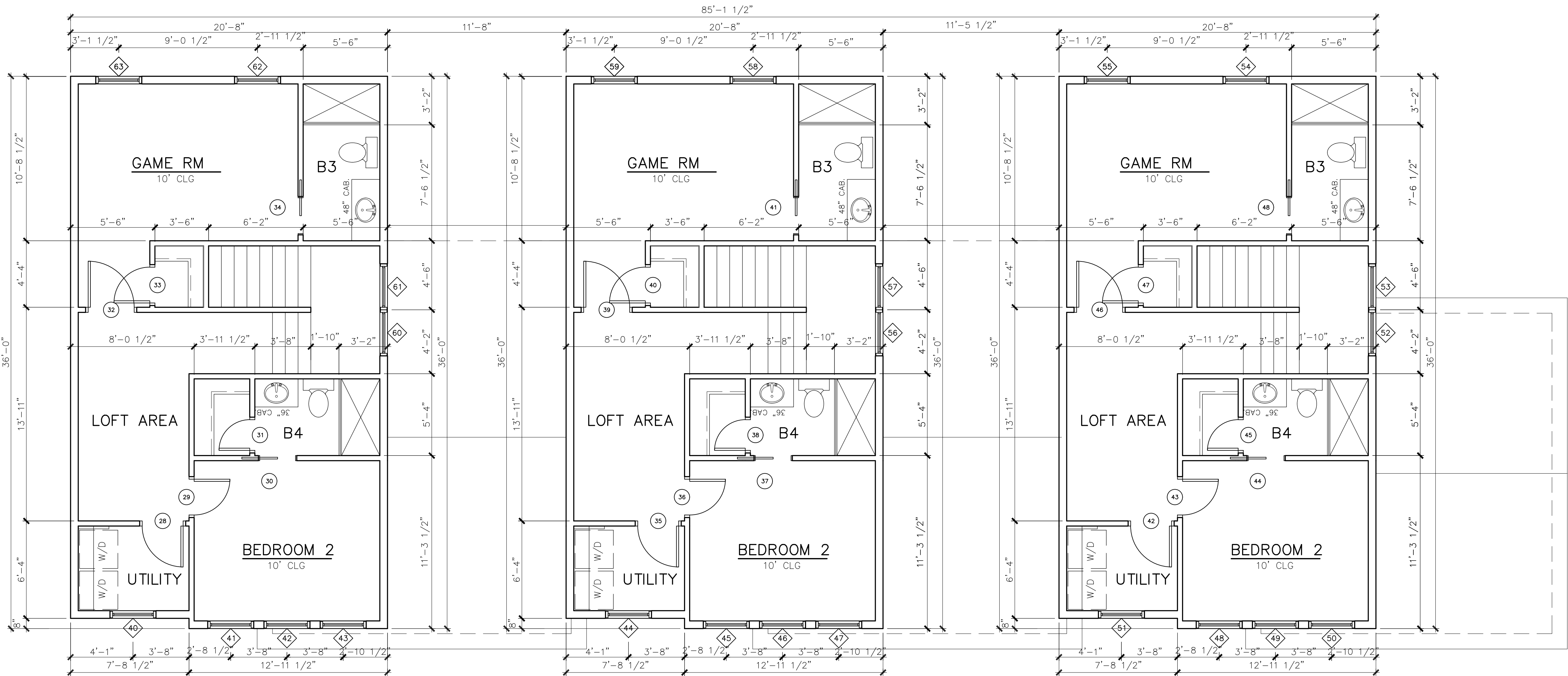
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PLAN NUMBER
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SHEET NUMBER

A-4



THIRD FLOOR PLAN

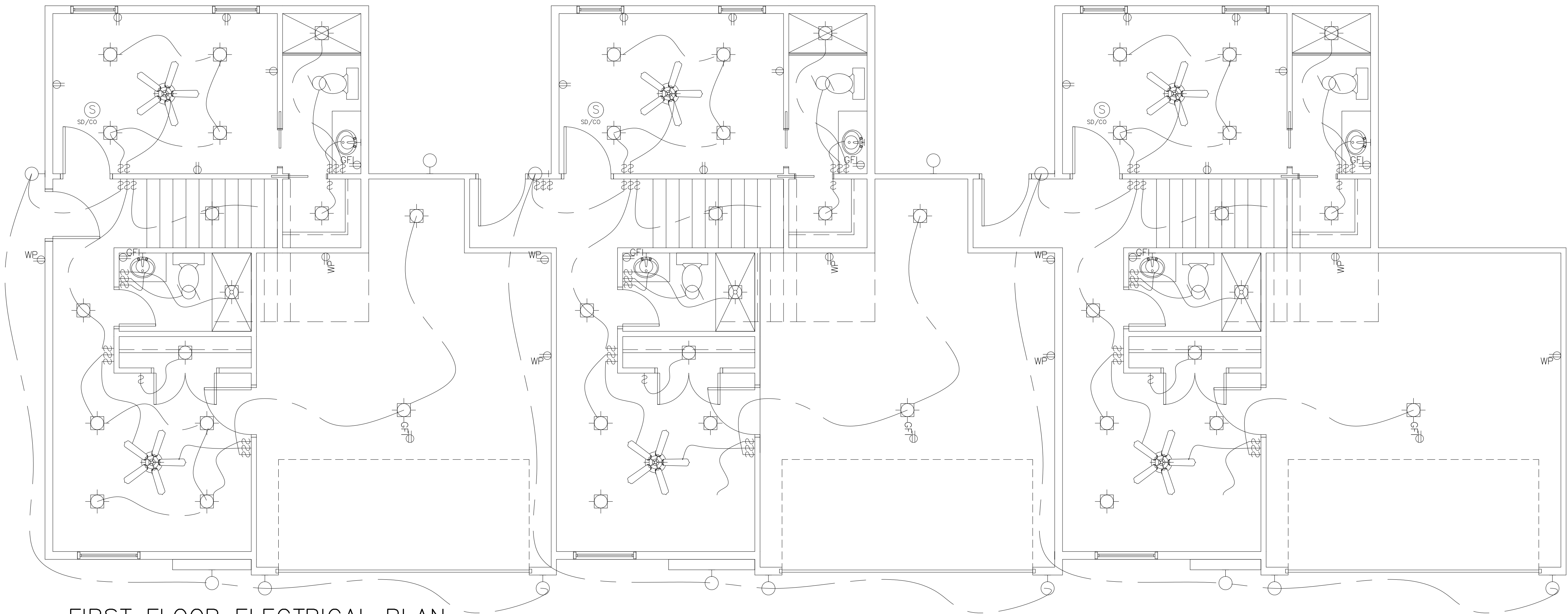
SCALE: 1/4" = 1'-0"

THIRD FLOOR PLAN

SCALE: 1/4" = 1'-0"

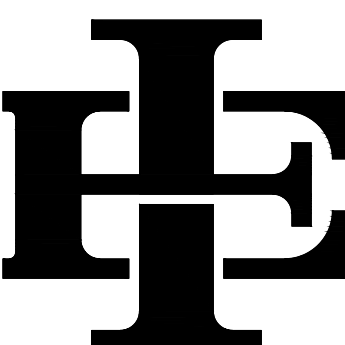
THIRD FLOOR PLAN

SCALE: 1/4" = 1'-0"



FIRST FLOOR ELECTRICAL PLAN

SCALE: 1/4" = 1'-0"



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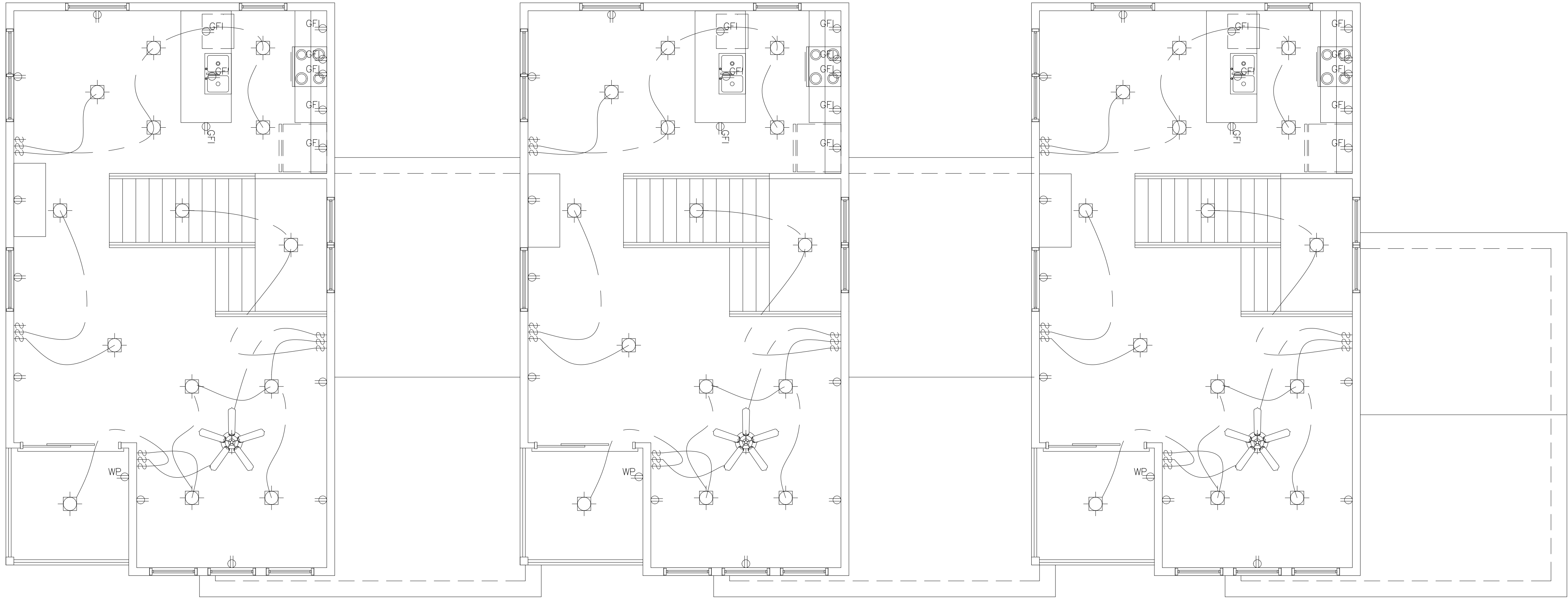
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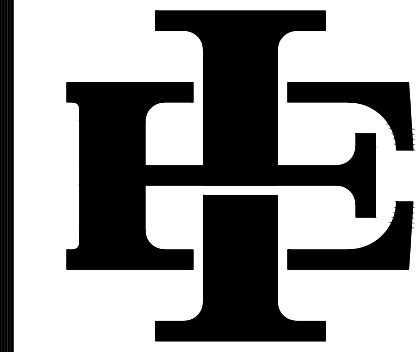
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SHEET NUMBER	A-5



SECOND FLOOR ELECTRICAL PLAN

SCALE: 1/4" = 1'-0"



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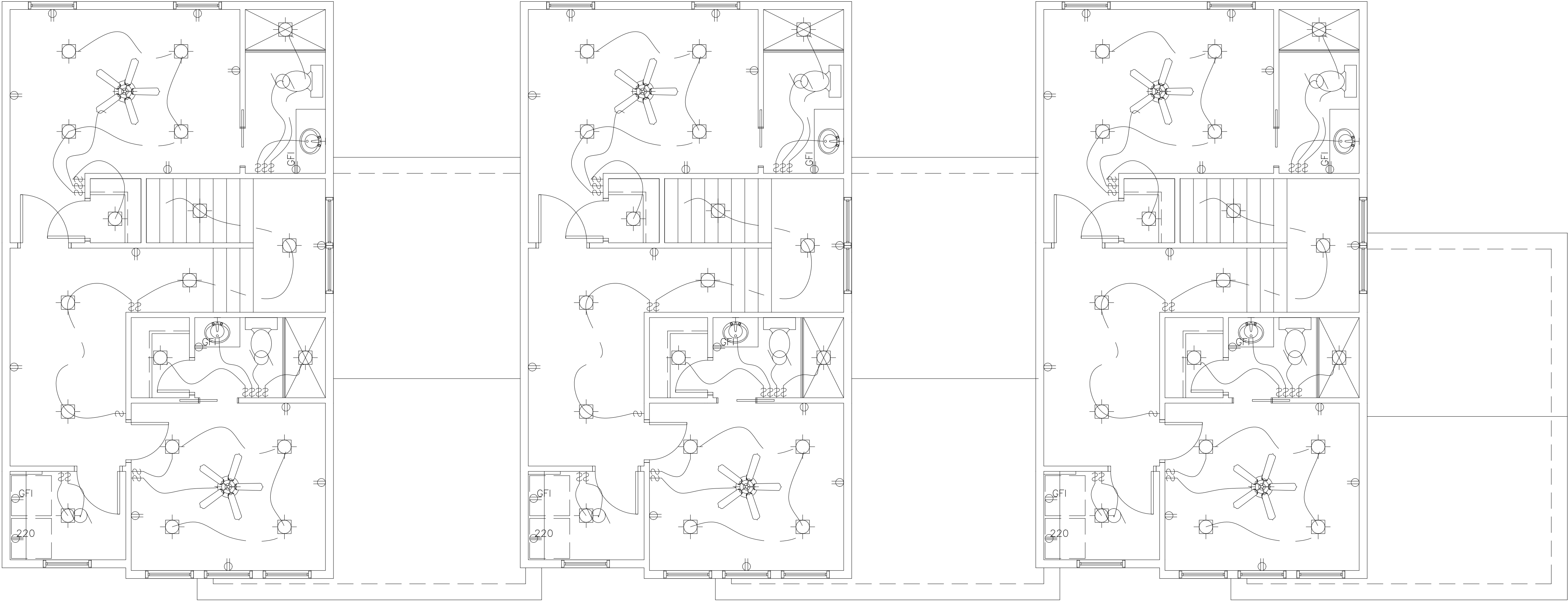
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MEM

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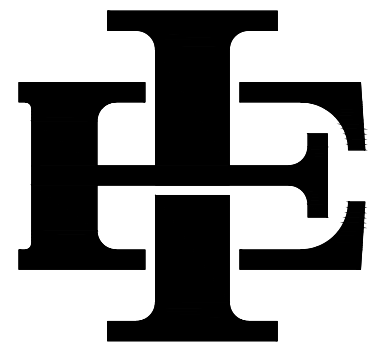
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SHEET NUMBER

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THIRD FLOOR ELECTRICAL PLAN
SCALE: 1/4" = 1'-0"



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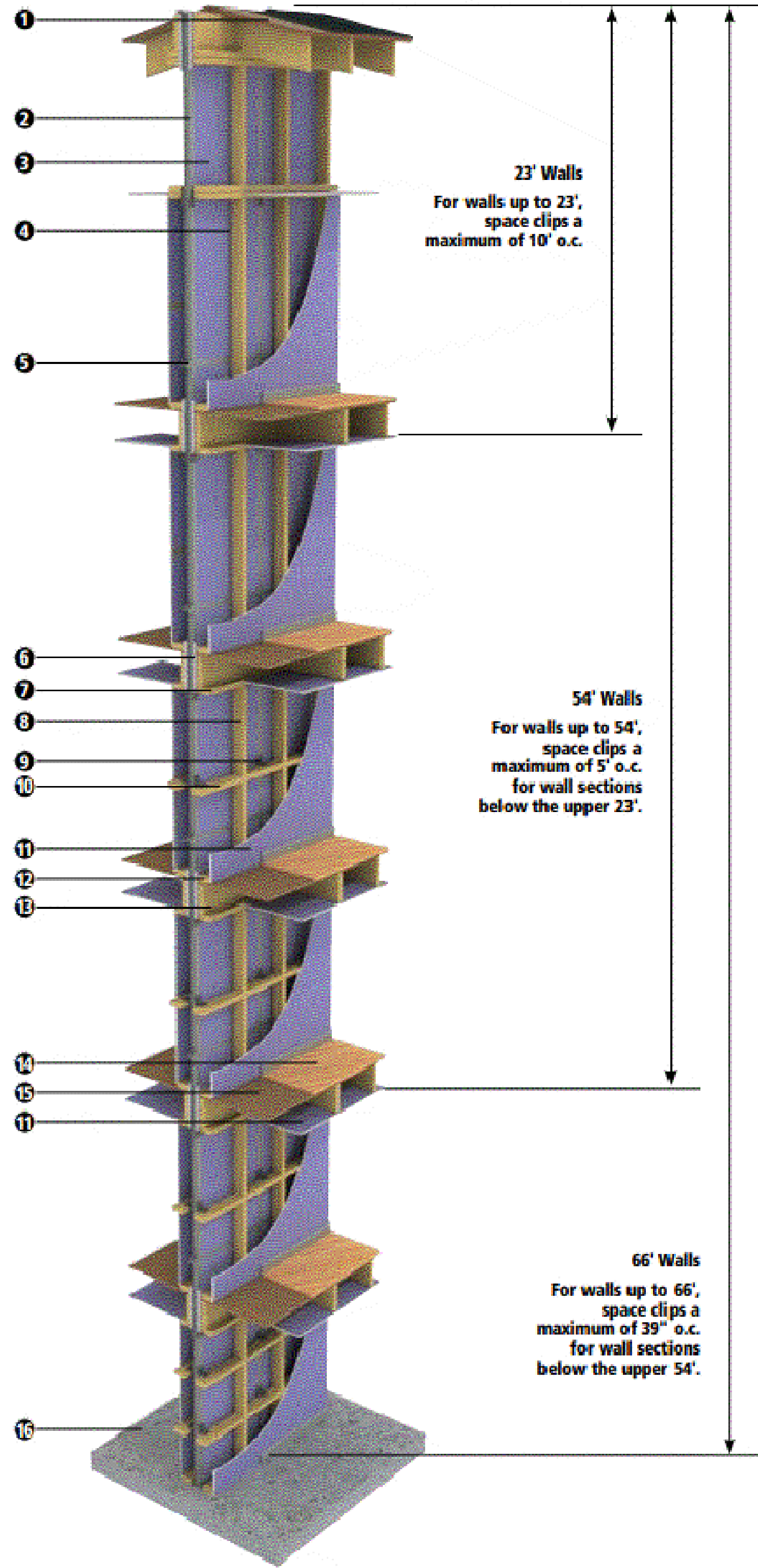
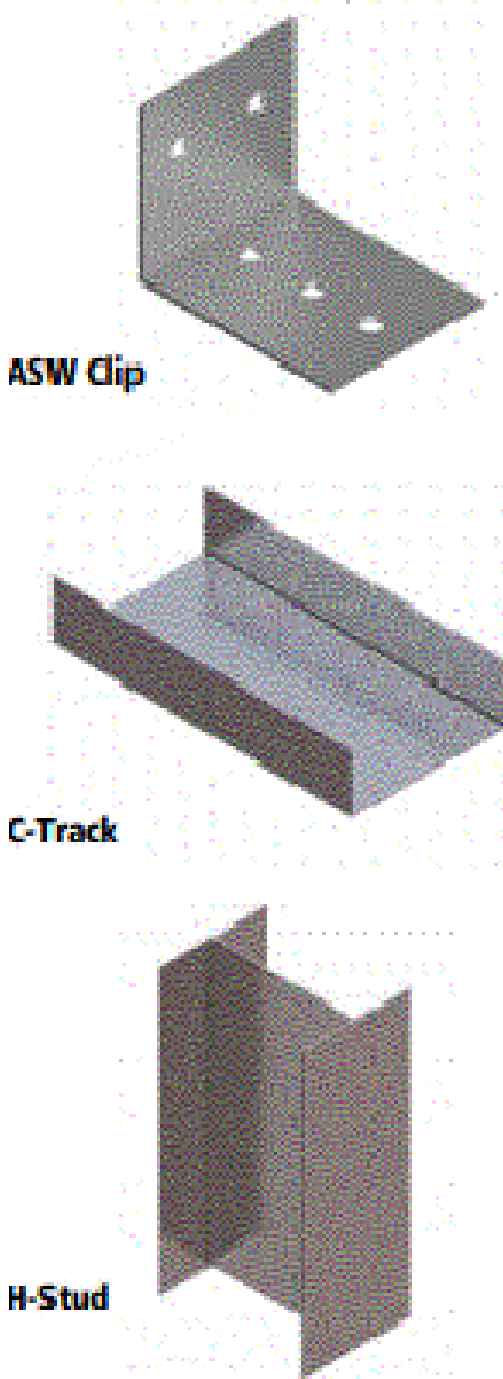
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PLAN NUMBER
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SHEET NUMBER

A-7

AREA SEPARATION WALL
LIMITING HEIGHTS

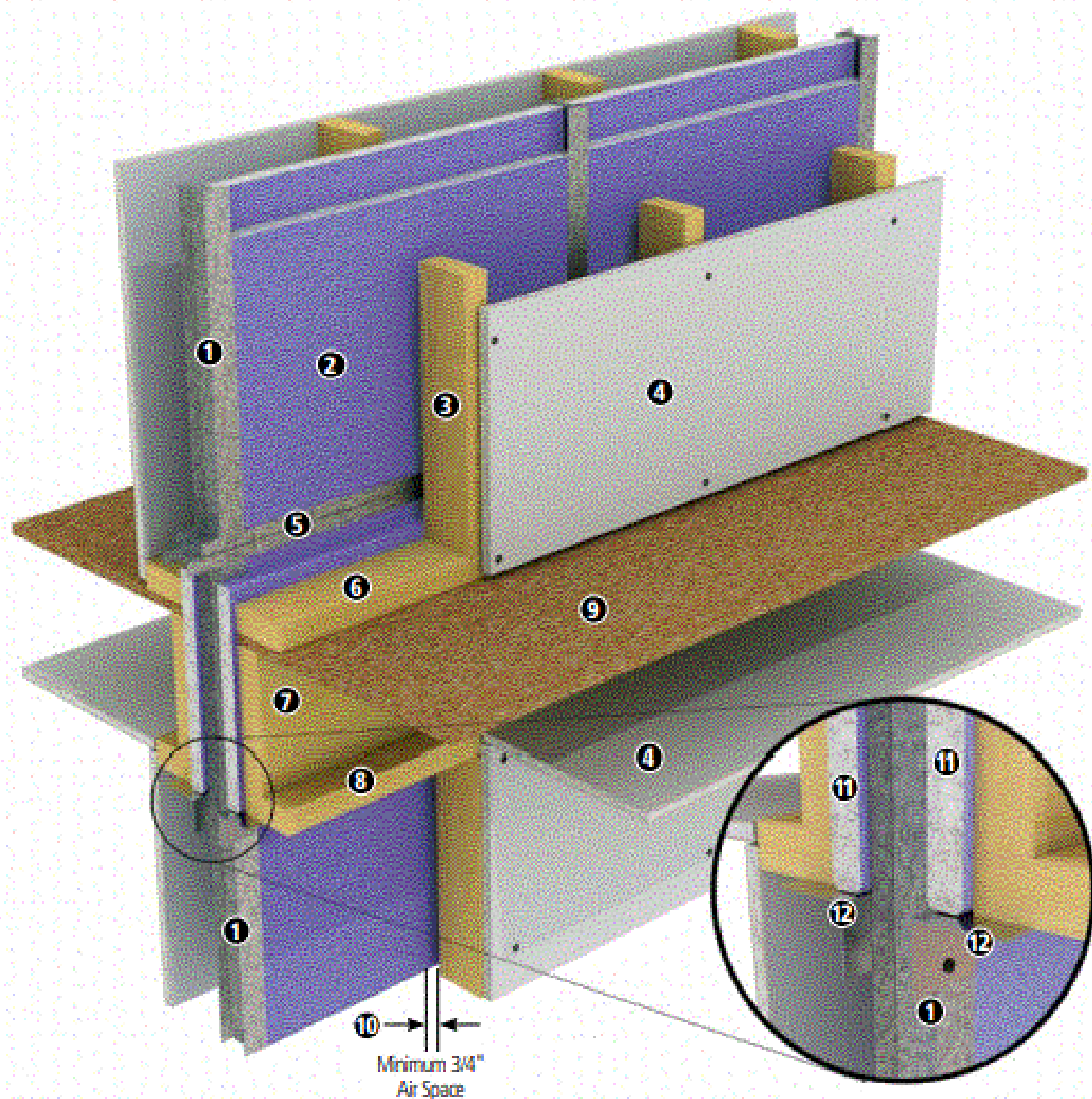
1. Roof
2. H-Stud
3. Two Layers 1" Shaftliner XP®
4. Minimum 3/4" Air Space
5. Double C-Track (Back-to-Back)
6. Fire Blocking
1" Shaftliner XP® or Mineral Wool
7. Top Plate
8. Stud
9. ASW Clip
10. Blocking
11. XP® Gypsum Board
12. Bottom Plate
13. Rim Joist
14. Finish Floor
15. Subfloor
16. Concrete Slab



The 2-Hour Area Separation Wall System is a 2-hour fire wall consisting of 2 in. (50.8 mm) light-gauge steel H-Studs that secure two layers of 1 in. (25.4 mm) shaftliner panels friction-fit between studs and a minimum 3/4 in. (19.1 mm) air space on each side.

TYPICAL FLOOR/
CEILING JUNCTURE

1. H-Stud
2. Two Layers 1" Shaftliner XP®
3. Stud
4. Gypsum Board
5. Double C-Track (Back to Back)
6. Bottom Plate
7. Rim Joist
8. Top Plate
9. Subfloor
10. Minimum 3/4" Air Space
11. Fire Blocking
1" Fire-Shield® Shaftliner
or Mineral Wool
12. ASW Clip



NATIONAL GYPSUM COMPANY
2-Hour Area Separation Wall System

Description

National Gypsum Company produces three shaftliner products for use in the Area Separation Wall System:

Gold Bond® mako Fire-Shield® Shaftliner consists of a fire-resistant Type X gypsum core encased in a heavy, moisture-resistant and green paper that is made from 100-percent recycled content.

Gold Bond® mako XP® Shaftliner consists of a mold-, mildew-, moisture- and fire-resistant Type X gypsum core with a specially designed PURPLE® fiberglass mat facer that offers superior resistance to mold and mildew.

Gold Bond® mako EXP® Shaftliner consists of a fire-resistant Type X gypsum core encased in a coated, specially designed PURPLE® fiberglass mat facer for superior mold, mildew and moisture resistance.

The steel H-Studs are attached on each side to adjacent framing with aluminum ASW break-away clips. The clips melt when exposed to heat and allow the collapse of the fire-exposed unit without failure of the area separation wall.

The H-Studs are secured at the foundation by the flanges of the C-Track. The same track is used back-to-back at intermediate floors to provide a splicing means so that the system can be erected one floor at a time. C-Tracks are also used at the roof line or at the parapet and at the ends of walls.

For a 2-hour, fire-rated assembly without the need for battens, maintain a minimum 3/4 in. (19.1 mm) air space between the H-Stud assembly and any adjacent framing members. When you cannot maintain a minimum 3/4 in. (19.1 mm) air space, cover the H-Studs and C-Tracks with gypsum board battens. In lieu of battens, fasten gypsum board to the H-Studs, and treat joints with tape and joint compound to provide a finished wall surface.

Wood- or steel-stud framing walls on each side of the area separation wall system can be load-bearing and can accommodate mechanical, electrical and plumbing systems. Install mineral wool or glass fiber insulation to provide higher STC ratings.

Technical Data

The Area Separation Wall System has a non-bearing wall rating of 2 hours and is listed in the UL Fire Resistance Directory as Design No. U387 and in the GA-600 Fire Resistance Manual as file numbers ASW 0800, ASW 0961 and ASW 0996.

The Area Separation Wall System has been evaluated for code compliance in UL Evaluation Report UL E8 R3591-01.

The Area Separation Wall System may be built up to a maximum of 66 ft. (20.1 m) high.

Do not use the Area Separation Wall System where exposure to constant dampness and/or water may occur.

Although steel framing and Gold Bond® mako EXP® Gypsum Panels can withstand temporary exposure to moisture during construction, protect the finished wall as soon as possible.

Protect insulation in the Area Separation Wall from getting wet. Do not install until the building is enclosed.

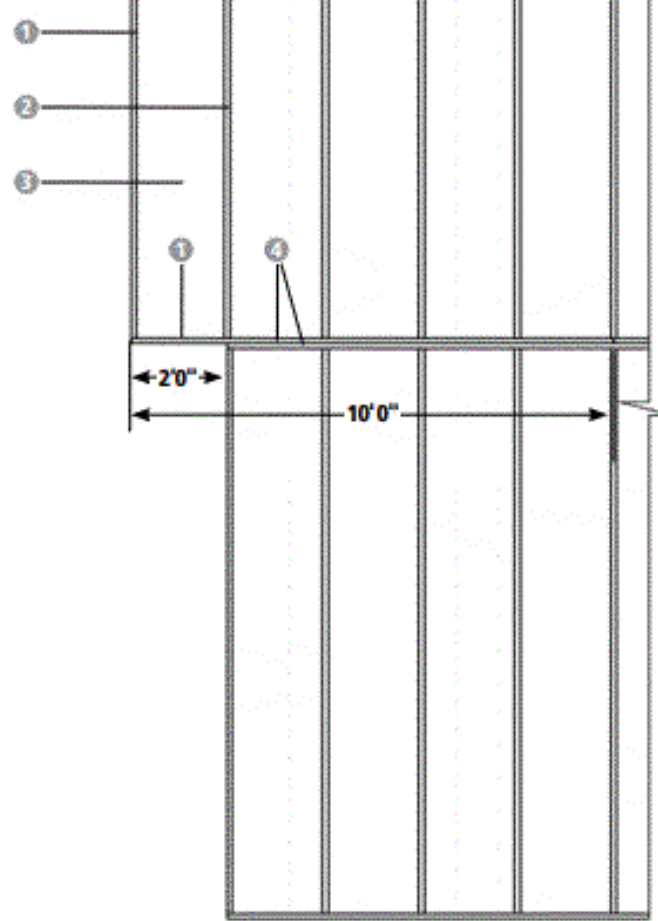
Properly store materials supplied to the jobsite, support off the ground, and protect from inclement weather.

Installation

1. Attach 2 in. (50.8 mm) C-Tracks to the top of the foundation 3/4 in. (19.1 mm) from the adjacent framed wall with fasteners spaced 24 in. (610 mm) o.c. Apply acoustical sealant along edges of track to minimize sound transmission.
2. Install C-Track on the ends of stepped foundation walls aligned with the Area Separation Wall end, if applicable, with fasteners 24 in. (610 mm) o.c. Caulk edges as with the floor track.
3. At the intersection of foundation and the exterior wall, begin erecting Area Separation Wall by inserting first layer of 1 in. (25.4 mm) shaftliner into C-Track. Insert second layer back-to-back with first layer and seat into C-Track. Shaftliner and studs may be set into position from the basement floor or fed down through the space provided between the wood framing from the floor above. Cap the terminating edge of the shaftliner panels with a vertical C-Track at the end of the foundation and fasten to the floor track with 3/8 in. (9.5 mm) Type S pan-head screws.

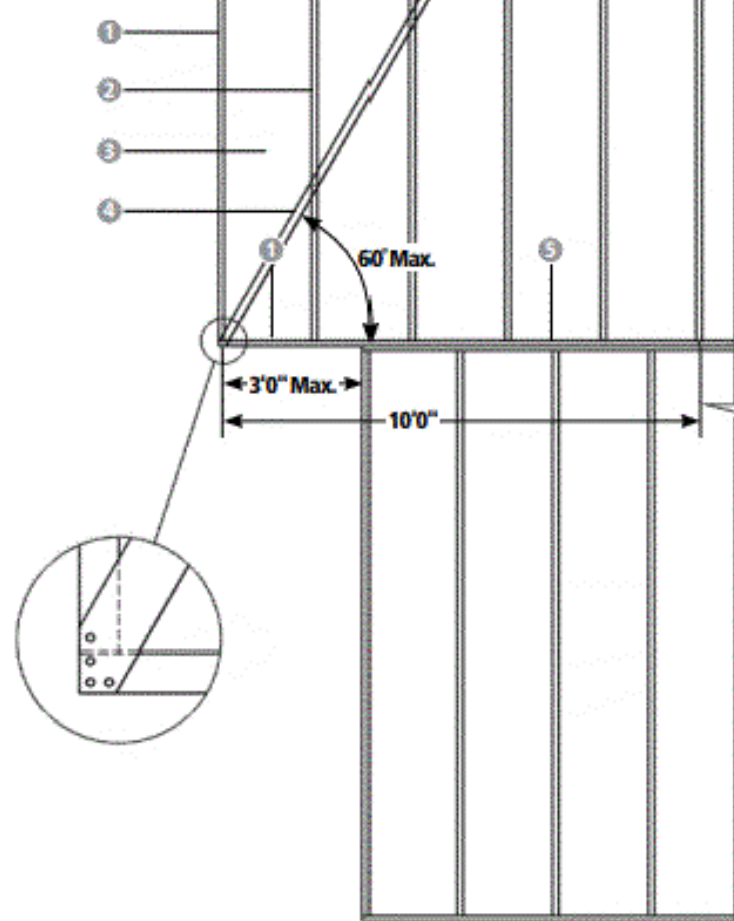
2 FT. CANTILEVER

1. 2" C-Track
2. 2" H-Stud
3. 1" Fire-Shield Shaftliner
4. Double C-Track (Back-to-Back)



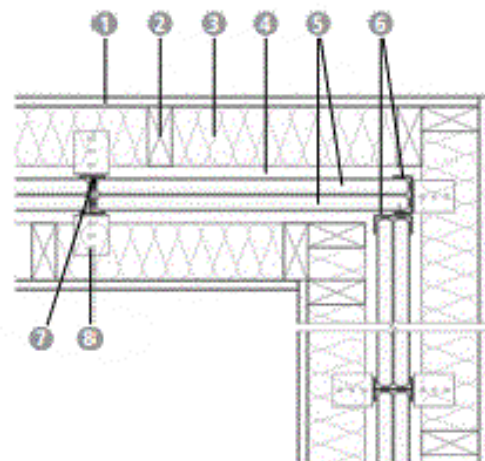
3 FT. CANTILEVER

1. 2" C-Track
2. 2" H-Stud
3. 1" Fire-Shield Shaftliner
4. Diagonal Steel Strap
5. Double C-Track (Back-to-Back)



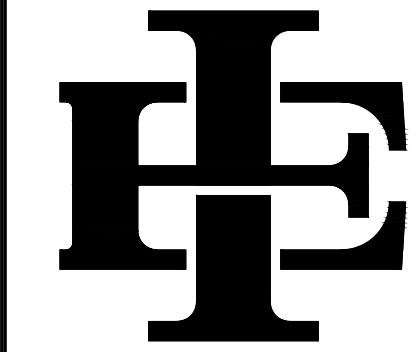
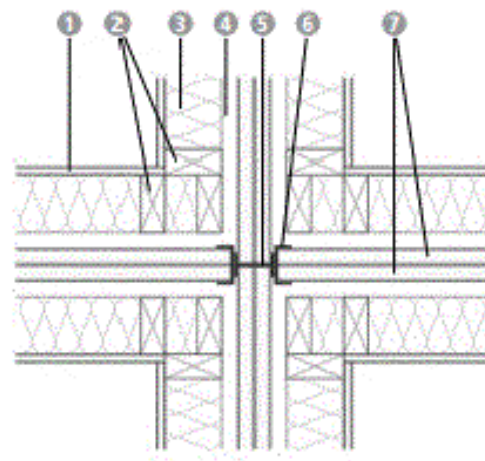
CORNER DETAIL

1. Gypsum Board
2. 2x4 Wood Stud
3. Insulation
4. Minimum 3/4" Air Space
5. 1" Fire-Shield Shaftliner
6. 2" C-Track
7. 2" H-Stud
8. ASW Clip



4-WAY INTERSECTION DETAIL

1. Gypsum Board
2. 2x4 Wood Stud
3. Insulation
4. Minimum 3/4" Air Space
5. 2" H-Stud
6. 2" C-Track
7. 1" Fire-Shield Shaftliner



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FORT WORTH, 76244
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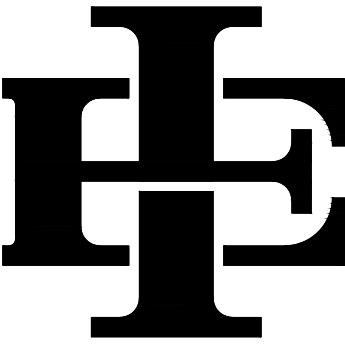
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SHEET NUMBER

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FRONT ELEVATIONS

SCALE: 1/4" = 1'-0"



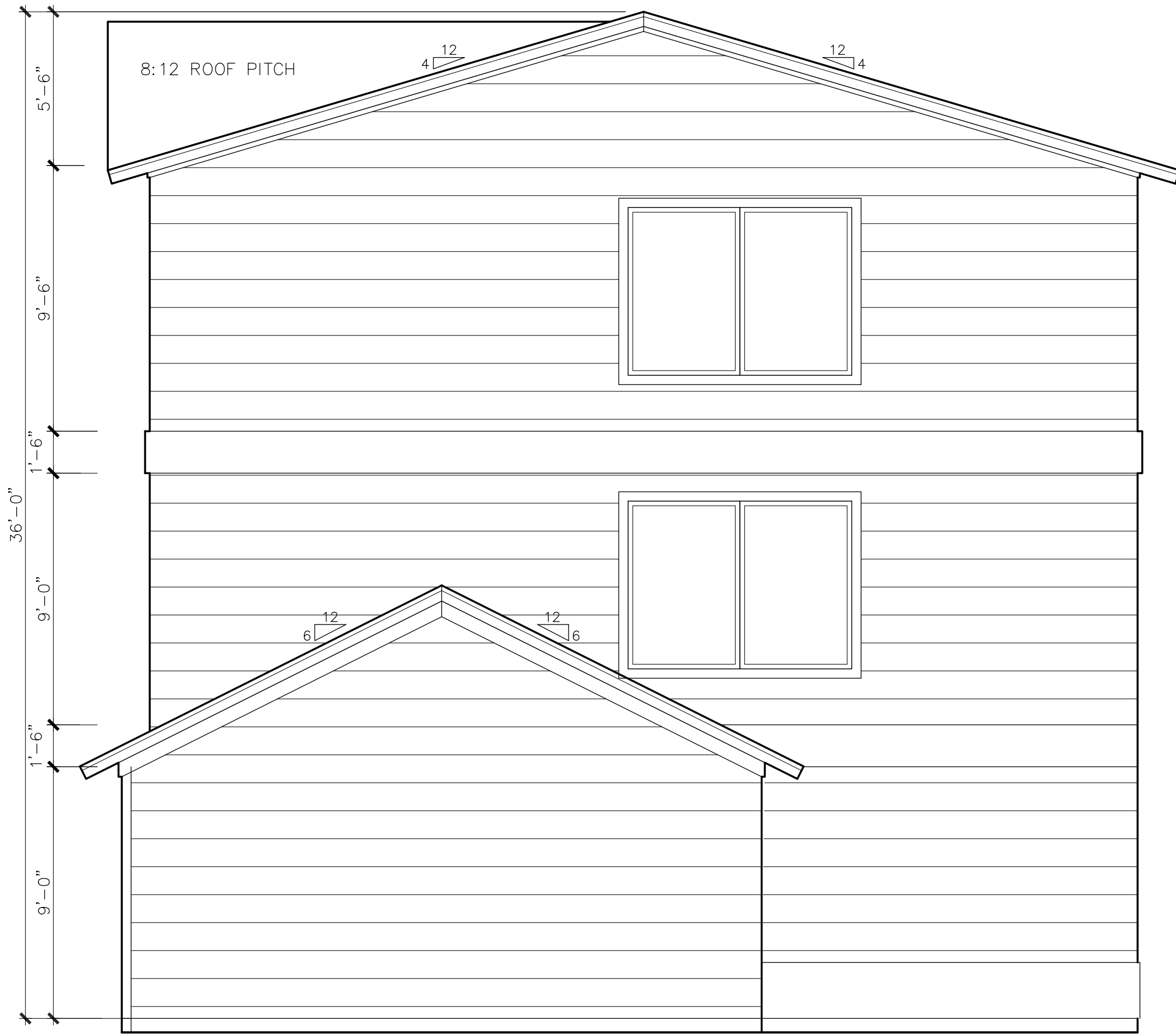
REAR ELEVATIONS

SCALE: 1/4" = 1'-0"



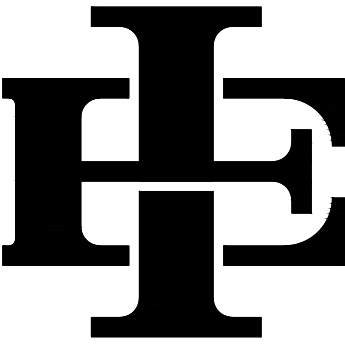
LEFT ELEVATIONS

SCALE: 1/4" = 1'-0"



RIGHT ELEVATIONS

SCALE: 1/4" = 1'-0"



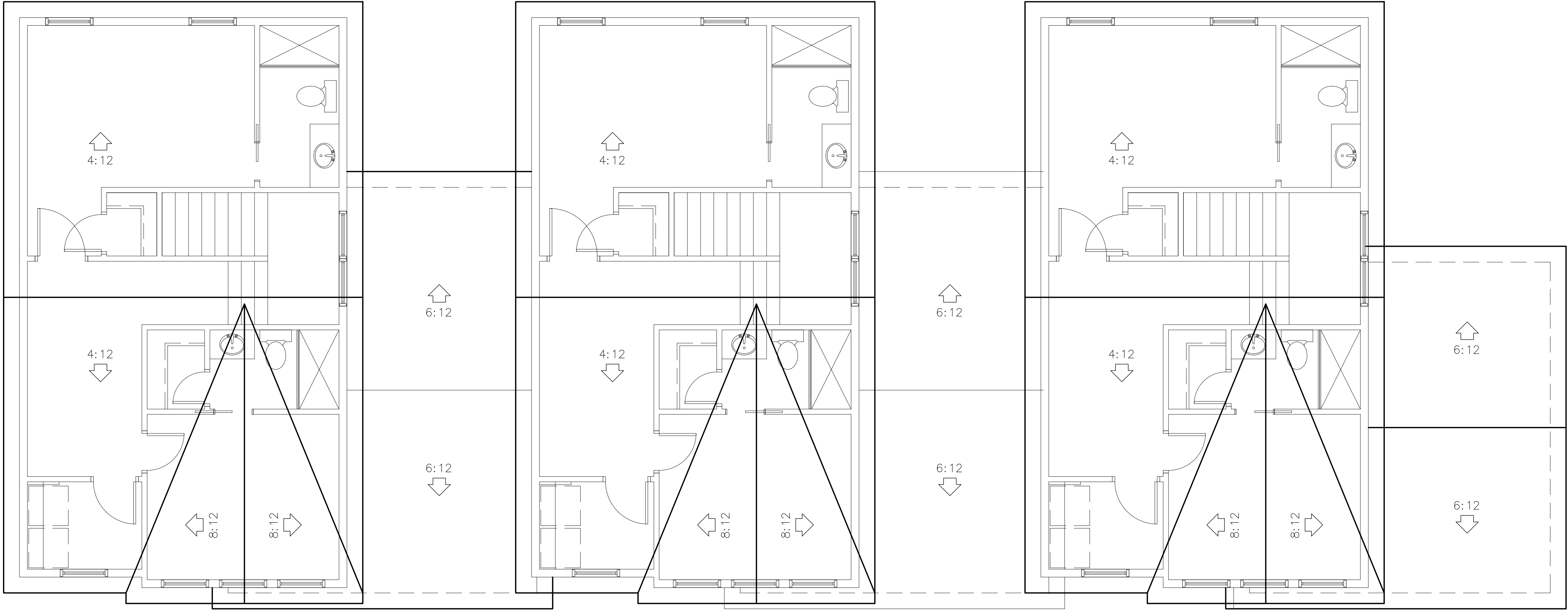
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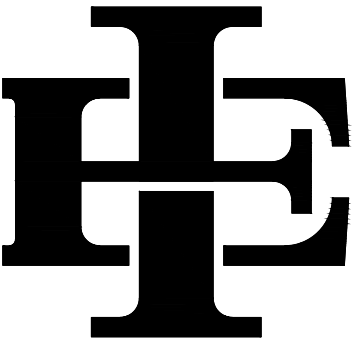
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ROOF PLAN

SCALE: 1/4" = 1'-0"



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